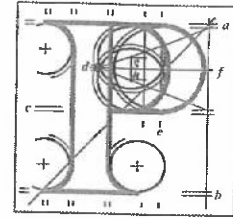


Our Case Number: ABP-314610-22

Planning Authority Reference Number:



**An
Bord
Pleanála**

Neasa Hourigan
Leinster House
Kildare Street
Dublin 2
D02 XR20

Date: 22 November 2022

Re: BusConnects Ballymun/Finglas to City Centre Core Bus Corridor Scheme
Ballymun/Finglas to Dublin City Centre

Dear Sir / Madam,

An Bord Pleanála has received your recent submission (including your fee of €50) in relation to the above-mentioned proposed road development and will take it into consideration in its determination of the matter.

Please note that the proposed road development shall not be carried out unless the Board has approved it or approved it with modifications.

If you have any queries in the mean time, please contact the undersigned. Please quote the above mentioned An Bord Pleanála reference number in any correspondence or telephone contact with the Board.

Yours faithfully,

Sarah Caulfield
Executive Officer
Direct Line: 01-8737287

HA02 (Acknowledgement - No Receipt to Issue)

Teil	Tel	(01) 858 8100
Glaó Áitiúil	LoCall	1890 275 175
Facs	Fax	(01) 872 2684
Láithreán Gréasáin	Website	www.pleanala.ie
Ríomhphost	Email	bord@pleanala.ie

64 Sráid Maoilbhríde	64 Marlborough Street
Baile Átha Cliath 1	Dublin 1
D01 V902	D01 V902

Neasa Hourigan TD
Leinster House
Kildare Street
Dublin 2
D02 XR20

15th November 2022

Re: Case number: 314610, BusConnects planning application

Dear Sir/Madam,

I wish to comment on the above planning application. Overall, I really welcome the plans and the prospect of the development of public transport within the Dublin Area. This is particularly important for the Dublin Central areas which I represent which have very low car ownership rates and therefore are particularly reliant on public and sustainable transport modes.

It is important that BusConnects is a step forward to increased access to safe transport, whether through buses or walking or cycling and an improved, healthier living environment with less noise and air pollution, less traffic and less speeding. I think careful consideration of the diversity of needs of communities, particularly those living in the inner city can achieve that.

There are a number of points, detailed below, that I would like addressed in the current designs.

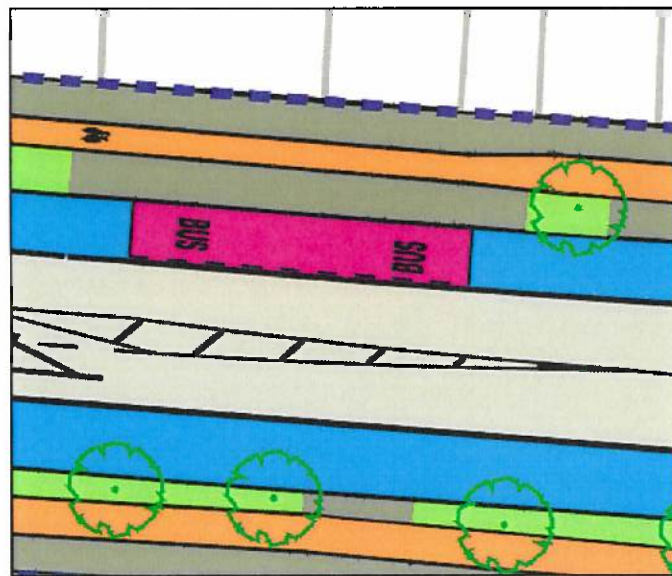
Kind Regards,



Neasa Hourigan TD, Dublin Central

1. Accessible bus stops

With a view to a street user hierarchy that would prioritise the more vulnerable footpath users (the disabled, the elderly, young children, etc.) we would ask that measures be implemented to ensure that bus stops can be accessed safely. This is especially important where the bus stop is only accessible by crossing a cycle lane. It is not clear from the designs what steps are currently being taken.

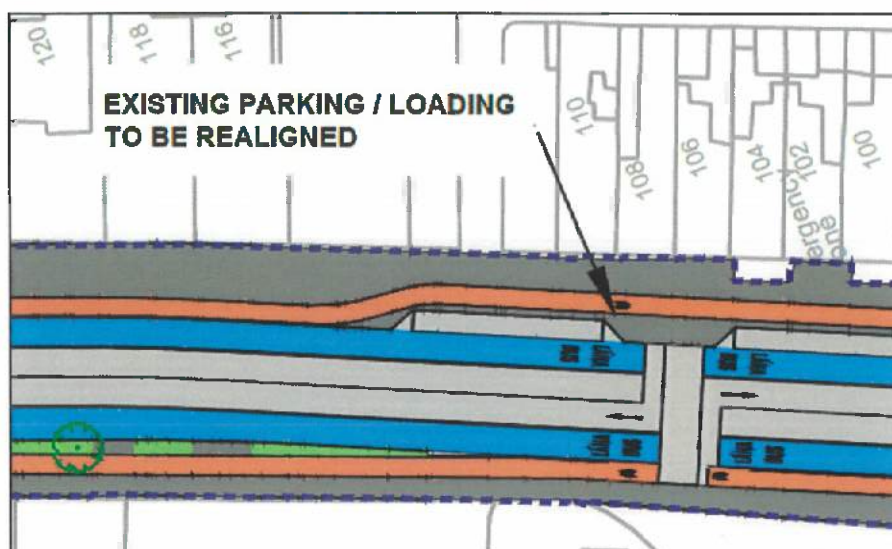


2. Accessible on street parking

In a similar vein we would like to know what measures are being taken to ensure the safety of cyclists and drivers using parking protected bike lanes.

Some drivers/passengers who will use the parking will be disabled and/or infirm and we should do our best to accommodate such drivers/passengers. We have seen parking protected bike lanes deployed in Dublin Central that appear to have given little regard for how people would safely exit the car (either cycle lane side or traffic side) or where people would cross the cycle lane to get to the footpath.

Likewise it needs to be clear for cyclists where they can expect pedestrians to cross the cycle lane.



3. Phibsborough

We note the previous submissions made by the local Beyond the Junction group and are in agreement with the issues that they identify, namely:

1. Motor traffic levels
2. The pinch points
3. Lack of provision for local businesses
4. No safe direct route to the city centre for cyclists

To focus specifically on two items we feel strongly that

- The 200-300 estimated change in road traffic with CBC in place (AM Peak 2028) on Connaught Street is a significant increase for a residential street that is already busy.
- Overall the plan means that Phibsborough village is being confined to being a junction and there is little scope to improve the village for residents, pedestrians, cyclists and local businesses.

5. Local diversion via Glasnevin Hill, Cremore Villas and Griffith Avenue.

We note that local traffic will now be diverted via Cremore Villas. We would ask that:

- Appropriate measures be required to ensure that only local traffic uses this route

-
- That existing on street parking on the east side of Cremore Villas is protected as residents in these dwellings do not have off street parking.